

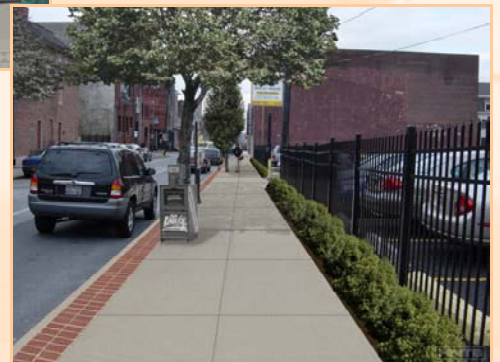
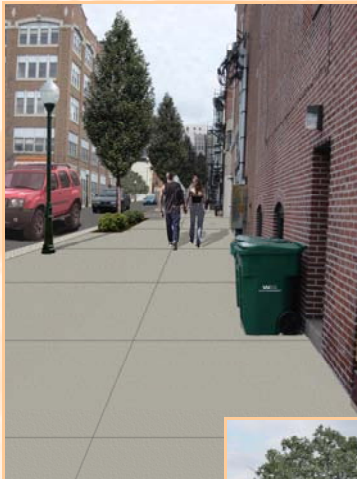
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In partnership with



Creating a Safe, Attractive and Viable Commercial and Residential Environment

Shipleigh Street Revitalization Plan



ShIPLEY Street Revitalization Plan

*Initial Observations and Process for Improving ShIPLEY Street
to Create a Safe, Attractive and Viable Commercial and Residential Environment*

Introduction and Goals 1

Current Use and Observations 1

Public Outreach Process..... 6

Recommendations..... 6

 Enhance Rear Building Entrances and Facades 6

 Improve Perceived Safety and Security Along Corridor 7

 Remove Obstacles to Pedestrians..... 7

 Bury Utilities..... 8

 Improve the Appearance of Vacant Buildings 8

 Install Consistent Streetscaping 9

 Minimize the Visual Impacts of Trash Storage Areas 9

 Encourage the Use of Public Art and Landscaping 10

 Minimize the Visual Impact of Parking..... 11

 Coordinate with Downtown Business Association..... 11

Implementation Plan..... 12

Appendices

- (A) Three concept plans for corridor streetscaping
- (B) Mapping of conditions including crash locations, transit routes, level of service, pedestrian counts, demographics and land use
- (C) Public comments

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Introduction and Goals

This project provides for public infrastructure improvements to support private commercial and residential development initiatives along Shipley Street from 10th Street to Martin Luther King Boulevard. The report has been prepared by the Wilmington Area Planning Council (WILMAPCO), at the request of the City of Wilmington, in partnership with the City and the Delaware Department of Transportation.

The economic vitality and the aesthetic quality of each business are important, and both are linked to the success of surrounding commercial establishments. Thoughtful design improvements often lead to greater sales for a business. Furthermore, the physical character of an area contributes greatly to its overall image for residents, customers, businesses, and visitors. By improving Shipley Street we hope to:

- Enhance the commercial success of the corridor by ensuring a pleasant experience for business patrons.
- Enhance the experience of pedestrians in the corridor.
- Preserve and enhance its historic buildings, streetscape, and architectural features.
- Achieve these goals affordably so that business and property owners are benefited rather than burdened by the revitalization process.

The report includes current use and observations with photos demonstrating existing conditions in the corridor, a description of the public outreach process, recommended improvements and an implementation plan. The appendices show:

- (A) Three concept plans for corridor streetscaping and mapping of addition known transportation
- (B) Mapping of conditions including crash locations, transit routes, level of service, pedestrian counts, demographics and land use
- (C) Public comments

Current Use and Observations

Shipley Street serves as both the “back door” for businesses fronting onto Market Street and as a potential economic development corridor for the City of Wilmington. For existing businesses, it functions as a service road, or alley. Trash collection and utilities are along the corridor, as well as parking for customers and employees. Yet, the supply of vacant buildings, available parking and central location potentially make this neglected street attractive to new businesses.

Currently, Shipley Street has many examples of beautiful architecture, landscaping and streetscaping. Restaurants, colleges, a church, and other businesses and nonprofit organizations are located on Shipley Street. The Community Services Building at the north end and the Ships Tavern District and Martin Luther King Boulevard at the south end are attractive anchors at either end of the study area. However, vacant buildings, exposed garbage cans, and deteriorating building facades and infrastructure make this street inhospitable for pedestrians, business clientele, and potential new businesses. A mix of paving and lighting treatments and conditions add to the area’s uncoordinated appearance.

The Community Service Building (below) and streetscaping near MLK Boulevard (right) contribute to appealing gateways to the corridor



Trash cans along Shipley Street are typically in full view (left) and sometimes even block the sidewalk, however some properties have enclosed trash storage to minimize their appearance (below and right)



Shipley Street Revitalization Plan



A variety of parking on Shipley Street includes the parking garage, surface parking and on street parking. The appearance of some parking has been improved with landscaping and decorative fencing.



Improvements to the Ships Tavern District (above) are improving the appearance of the 200 block of Shipley Street, adding upscale homes and retail, and creating a gateway to the corridor.

ShIPLEY Street Revitalization Plan



Springfield College (above) and Delaware Technical and Community College (top, right), located on Shipley Street, bring the potential for a lively campus atmosphere with restaurants and retail to serve students (right).



Concentrations of vacant, disbeveled or boarded up buildings can make pedestrians feel unsafe.



Shipley Street Revitalization Plan



Attractive storefronts in sections of Shipley Street create an inviting environment.



Streetscaping has been installed in sections of the corridor, but some has fallen into disrepair.



Public Outreach Process

Recommendations were developed based on the observations of the study team and stakeholder input received throughout the process. We began our outreach process by mailing stakeholders a project fact sheet and survey. The survey provided a quick, easy way for people to share their ideas, and could be completed on the project web site or returned via mail or fax. At the “walking tour” on January 7, 2004 stakeholders reviewed existing conditions and pointed out specific areas of concern; tour was held indoors due to the cold weather that day. Interviews gave us another way to sit down and learn people’s ideas and concerns. The open house design workshop was held January 28, and allowed people to drop by at their convenience between 10 a.m. and 5 p.m.

Recommendations

Improvements along Shipley Street must be carried out through a public/private partnership with private façade improvements supported by public safety and streetscape improvements and incentives for property owners and businesses.

Enhance Rear Building Entrances and Facades

The rear entrances to buildings between Shipley and Market streets may be the primary entrance for those parking along Shipley Street. Improvements to rear entrances should be encouraged, such as signs, landscaping, and complementary architectural detailing, to create a welcoming and safe environment.

Similarly, the facades of buildings along Shipley Street should be enhanced. Currently, many beautiful, historic buildings are located along the corridor. These buildings should be preserved, and architectural elements of these should be incorporated in other buildings. Attractive business fronts should have abundant windows, attractive lighting, and accessories such as awnings, flags or plantings.

The City should work with business owners to identify grants or low interest loans to assist in implementing façade improvements. Eligible participants should include property owners and business lessees, possibly with preference given to planned or existing mixed-use residential/commercial development. Eligible improvements should be visible from publicly owned space. These might include:

- Rehabilitate, restore or repair exterior
- Install, enlarge, or repair windows
- Install exterior lighting
- Screen mechanical equipment or trash disposal
- Paint exterior including murals
- Clean exterior facade
- Construct permanent landscaping
- Install or upgrade awnings and signage
- Provide pedestrian-oriented signage
- Architect fees
- Americans with Disabilities Act (ADA) improvements that are an integral part of a facade improvement proposal



Façade improvements at parking garage might include brick façade, murals, plantings, and a new sign.

Improve Perceived Safety and Security Along Corridor

Concerns about crime and personal safety, whether real or perceived, discourage people from visiting, living in, or opening businesses in Downtown Wilmington. A lack of pedestrian lighting and limited patrolling by the police/Clean & Safe Team makes this area feel unsafe at night. Some participants in our meetings reported that they had been victims of crime along Shipley Street. Research has shown that lighting is an effective method of reducing crime, and some studies have shown more than a 40 percent reduction after the installation of street lights¹. Equally important, research has found that the mere presence of street lights reduces the fear of crime². This increased sense of security was found to encourage more use of the street and reduce crime rates even during daylight hours.

Thus, we recommend that street lights be installed throughout the study area, consistent with ones in other areas of the CBD. Extending the hours of Clean & Safe or police patrols is also recommended, particularly during the winter when employees are likely to be arriving and departing after dark.



Remove Obstacles to Pedestrians

Obstacles along the sidewalk creates a challenging experience for pedestrians, particularly those with disabilities. Where feasible sidewalk cellar doors should be installed over basement hatchways, rather than railing. Existing pipe railings and plywood edging should be removed and replaced with a consistent decorative railing style, where it is not feasible to install flat covers. All railings should reflect the architecture of the building.

Street, wayfinding and other public signs should be consolidated and redundant signs should be removed. In some locations, mounting street name signs on building walls may be better than putting them on posts.

The City may wish to consider establishing an on-street parking system using centralized pay stations as an alternative to meters. If pay stations are used, they should be placed so that there is one station for every eight meters. By placing stations at the midpoint of the eight spaces, patrons would need to walk no more than four places to make payment. Pay stations offer the advantage of removing the clutter of individual parking meters; however, community opposition is likely unless the City invests in marketing to gain acceptance. Pay stations also provide customers the convenience of accepting coins, credit/debit cards or smart cards.



Existing conditions (left) are cluttered with pipe railings, plywood edges, parking meters and utilities. The more desirable conditions on the right have fewer obstacles to pedestrians, with vault entrances covered with cellar doors and removed parking meters. This area could be further improved by hiding exposed utilities on the building and improving the facades.



¹ National Crime Prevention Council - www.npc.org

² *The Influence of Street Lighting on Crime and Fear of Crimes and Effects of improved street lighting on crime: a systematic review*, London Crime Prevention Unit - www.homeoffice.gov.uk

Bury Utilities

Long term, the city should consider burying utilities to minimize their visual impact and unclutter the sidewalks. Undergrounding utilities would bury overhead wires, though transformers may need to remain above ground at street level. With fewer utility poles, the area would have better pedestrian access and more space for street trees if desired. Maintenance costs may also be less for buried utilities, though only a slight prevention in power outages due to storms or vehicular crashes is gained.

The primary challenge to burying utilities is the substantial capital cost. Costs range from \$500,000 to \$3 million per mile, varying substantially based on terrain, right of ways, number of service providers, type of utilities, conflicting underground uses, excavation costs, engineering costs and conflicts with other existing infrastructure. The costs can be minimized if work is done in conjunction with other streetscape improvements such as repaving or sidewalks. Other disadvantages are that customers must reconnect to new underground cables and distribution problems in the underground system may be more difficult to locate.

An alternative to burying utilities is to reduce the visual impact of above ground utilities. The appearance of utility poles and equipment such as substations, meters, and transformers can be screened or the city can work with utilities to replace poles with ones that complement the surroundings. Wires are already mostly on the west side of Shipley, but can be further consolidated and wrapped to create the appearance of only one cable. Using heavier cables and stronger poles can also reduce the number of poles required. Finally, raising wires above sight lines along buildings and signs can make them less noticeable.

Improve the Appearance of Vacant Buildings

Building occupancy indicates an area's economic prosperity or decline. When windows are vacant or boarded up, visitors assume the area is in a state of decline, and hence feel unsafe. Badly maintained vacant buildings may also discourage potential tenants and reinvestment, are a fire hazard and may attract crime. The primary emphasis should be on developing vacant properties. The International City/County Management Association recommends a 5-stage cycle to self-assess vacant property programs and gauge how to best allocate resources:



Source: International City/County Management Association

- **Prevention & Assessment:** Combine work with property owners, with pro-active code enforcement and develop a property information system.
- **Stabilization:** Use local code enforcement powers, both criminal and administrative.
- **Rehabilitation:** Provide financial resources and technical assistance.
- **Property Transfer or Acquisition:** Tax delinquency, land banks, and eminent domain.
- **Neighborhood Planning for Long Term Revitalization:** Comprehensive and community revitalization plans, affordable housing policies, and rebuilding markets. Other revitalization opportunities such as infill development, affordable housing, work force housing, livable neighborhoods.

The City may wish to designate a Vacant Property Review Commission. This appointed body would help identify “blighted properties.” These include properties which are a fire hazard, or is otherwise dangerous, are unfit for their intended use, or has been tax delinquent for a period of two years.

Until redevelopment occurs, emphasis should be placed on making vacant buildings appear “alive” and inhabited. Buildings should be maintained in such a condition as to improve community appearance, preserve property values and protect the safety and health of the city residents during the periods of their vacancy. Consider design guidelines that prohibit boarding up ground floor windows. As a low-cost alternative, encourage:

- Using windows of vacant buildings as temporary display space for local arts groups, school art programs, historic information, and/or community organizations.
- Keeping furniture, lights, blinds and/or merchandise in retail windows

When a vacant building is demolished and adjacent walls are exposed, install temporary or permanent treatments to improve the appearance of the visible surface. This might include use of masonry paint, murals, ivy or vines, or stucco.

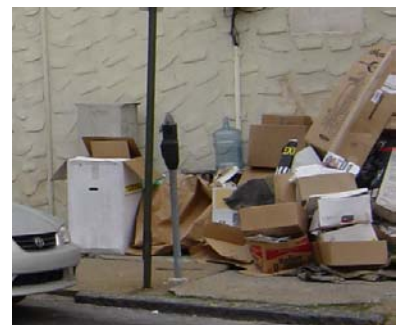
In addition to bringing the appearance of life to entirely vacant buildings, many occupied buildings should be fully utilized, filling vacant space upstairs with apartments or offices. With the recent success of other downtown housing combined with the proposed infrastructure improvements, these vacant spaces will likely become too valuable to stay empty and with time may be redeveloped by private parties. However, the cost of rehabilitation, particularly from office to residential, can be daunting. Therefore, it is preferable that the City or a downtown development corporation play a leadership role to accelerate this process. To fund refurbishment of vacant and underused buildings, the City should work with HUD, the State of Delaware and other agencies to provide low interest loans or grants for community and economic development. New Jersey’s “Upstairs Downtown Program” is one example of a successful program. An incentive similar to the “Christiana Gateway Tax Incentive Program” might also be considered, where businesses located or relocating to the area may be eligible for an abatement from property tax for the increased value that the property derives from renovations of existing structures or construction of new ones. Through a proactive program, the City can provide incentives for providing affordable rents to small business or bringing more living space downtown. Wilmington should also seek the assistance of the Delaware Economic Development Office in the marketing of available office space.

Install Consistent Streetscaping

Through the Wilmington Initiatives partnership, distinctive and attractive streetscaping has been installed throughout the downtown. Improvements with this same style should be extended to Shipley Street, particularly the addition of the lighting fixtures and highly visible crosswalks. Alternative concepts are detailed in Appendix C.

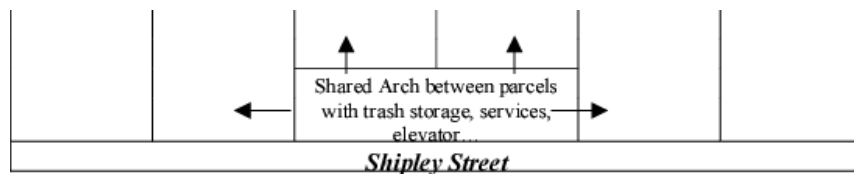
Minimize the Visual Impacts of Trash Storage Areas

Trashcans, bags, and occasionally piles of trash are visible along Shipley Street. Thus, a more consistent standard of collection and storage along with strict enforcement is crucial. Currently trash collection is done through a private contractors offering a variety of collection frequencies at, according to some businesses owners interviewed, high prices for often poor service. The City may wish to take over trash collection in the CBD to improve the quality of service businesses receive and reduce the burdensome fees. An innovative trash collection program used in Toronto sells “yellow bags” to small businesses at a cost that covers the city’s expense, thus encouraging waste reduction and offering a fair and affordable service. Similar “pay as you throw” systems are used in North American and European cities.



Some properties were found to store trash inside or in enclosures, but most place it in cans or loose on the sidewalk. On days when trash collection occurs, businesses should set their containers out at the curb, not in the middle of the sidewalk or in the street. Policy should be adopted that requires containers to be set out no earlier than 6 p.m. and removed from public view (either placed inside or concealed in an enclosure) by 10:00 a.m. The City should work with trash collection contractors to ensure they run their service within the designated hours and should enforce the adopted regulation.

Trash enclosures should be constructed with materials similar to those of the building or the streetscaping. The trash screen must be scaled for pedestrian traffic, and when feasible, should be shared by multiple properties. A possible solution to visible trash would be the establishment of shared-use enclosures, either on public property or in shared arches set up between adjoining parcels. Placed in parking lots, public enclosures could contain a compactor and would occupy about one parking space. Shared enclosures, as pictured on the right, could serve four adjacent parcels.



Encourage the Use of Public Art and Landscaping

Public art is a cost effective way to improve an area's image. One idea for improvement is a mural on the large black wall of the parking garage. Experts say blank walls create a hostile environment for pedestrians; a mural could easily address this.

Enhancing the park would also improve the area. Currently the park contains little greenery and has become a hangout for homeless people. While public art is generally a good thing, in this case the existing sculpture is out of scale with the size of the park and should be replaced with something more suitable. Desirable art for the park might be an historic themed mural done in partnership with the Delaware College of Art and Design. Existing park benches should be replaced with ones that discourage loitering and sleeping. Perhaps a mosaic chess table could be made for the park by local art students, to give people a positive reason to loiter.



Additional street trees should be considered along portions of Shipley Street, including the 200 block behind Del Tech, the 400 block to screen existing open space, and the 600 and 700 blocks to screen surface parking. Street trees can increase property values, help mitigate air pollution, slow storm water runoff and create a psychologically and aesthetically pleasing environment. Challenges are that trees can damage sidewalks, block business signs and storefront windows if poorly chosen or placed, and will require ongoing maintenance.

Minimize the Visual Impact of Parking

Where space permits, interior landscaping in parking lots greatly improves their appearance. All parking should be paved and screened from view; screening can be done with landscaping and/or ornamental fencing. Having surface parking adjacent to sidewalks without a defined edges is both unattractive and dangerous to pedestrians. The façade of the parking garage should be improved with a mural or some other decorative treatment. Long term, surface parking lots present an opportunity for infill development; this can be done with ground level retail incorporated into a parking structure or located in front of surface parking. Future parking structures should be designed to blend in with attractive architectural elements in the corridor such as the example on the right.



Retail wrapped around a parking structure.

Coordinate with Downtown Business Association

ShIPLEY Street is a lifeline for businesses using it as a service entrance for deliveries, trash collection and other services. Thus, it is crucial that access to ShIPLEY be maintained to minimize the financial impact of reconstruction or repaving on businesses. The following procedures, developed by the Downtown Business Association (DBA), should be followed:

Communication

- The city and/or contractors will provide written notice to all business and property owners one week prior to any disruption of utilities with schedule of times and dates, except in case of emergency.
- The city and/or contractors will provide written notice to all business and property owners one week prior to any street closure and any parking restrictions.
- A public meeting will be held prior to each major phase of work.

Logistics

- “No Parking” signage will be used only as needed and will be removed promptly upon expirations.
- Any barriers placed on sidewalks will be created to direct an orderly flow of pedestrian traffic.
- Construction workers will not be permitted to park at any open metered, non-metered and/or loading zone within the CBD. The contractors will notify their workers on this policy. City parking meter officer will aggressively monitor parking violations.
- Side streets will not be used as parking zone for construction equipment.
- Proper procedures to reduce the amount of dust will be implemented taking into consideration that many of the businesses are restaurants.
- The timely re-installation of signage and parking meters will occur to discourage freeloading parkers.
- When sidewalks are closed, signage will be displayed that directs pedestrians to alternate entrances to businesses.

Business Support

- A construction liaison will be identified from the project management side. This liaison will work with the DBA to communicate information to the merchants as needed.
- The DBA will identify block captains that will work with the merchants and serve as a communication conduit to the construction liaison.
- The DBA block captains and liaison will have briefings as needed to share information from both sides of the project.

- The DBA will present updates on the construction at general membership meetings.
- The DBA, through the SBA, SBDC and WEDCO, will offer financial and business consultation to affected merchants. This will include possible aid in negotiating leases, short term loans, etc.

Implementation Plan

Through a series of short and long term actions done in partnership with businesses, property owners and residents along the corridor, Shipley Street can become an area of pride for the City. Actions have been groups by intermediate, short term and long term measures with high, medium and low priority. High priority actions directly relate to the health, safety and security of the corridor as well as its economic vitality. Medium and low priority actions will greatly enhance the appearance and functionality of the corridor, but some may require additional study or are challenging to implement due to cost or organizational constraints. WILMAPCO looks forward to working with the City to implement this plan. Detailed cost estimates and concepts are in Appendix C.

	Action	Responsible Party	Priority	Comments
Immediate	<i>Enhance rear building entrances</i>			
	Beautify rear entrance and façade	Property/business owners	Medium	Flower pots, banners, awnings, fresh paint and other inexpensive measures to brighten entrances and facades.
	<i>Improve perceived safety and security along corridor</i>			
	Increase patrolling by police and Clean & Safe Team	Wilmington/Downtown Visions	High	This action should be done to address reported crime in the corridor.
	<i>Remove obstacles to pedestrians</i>			
	Audit and consolidate existing signs	Wilmington	Medium	Review street sign locations, consolidate signs onto single posts, and remove redundant or inappropriate signs.
	<i>Improve the appearance of vacant buildings</i>			
	Enliven vacant building windows with temporary displays	Wilmington/property owners/art and historic organizations	Medium	No capital cost. Assign task to area art and historic organizations.
	Address any immediate public health concerns from vacant properties	Wilmington/property owners	High	Inspect vacant buildings that have not been visited recently and identify any immediate safety concerns.
	Increase marketing of usable vacant space	Wilmington/property owners/DEDO	High	Not all vacant commercial properties appear to be listed on DEDO and Wilmington web site listing. Review listing and add properties not included.
	<i>Minimize the visual impact of trash storage</i>			
	Enforce existing litter control laws and enact new laws as needed	Wilmington	High	No enforcement appears to be taking place. Loose litter is a visual eyesore and a public health risk.
	<i>Coordinate with Downtown Business Association</i>			
	Coordinate with DBA on improvements	Wilmington/DBA	High	Notify DBA of actions taking place and consult them in decision-making when appropriate.

Short term (current-5 years)

Action	Responsible Party	Priority	Comments
Enhance rear building entrances			
Improve rear entrance and façade	Property/business owners	Medium	Expand on immediate entrance and façade improvements made, including lighting, permanent landscaping, decorative façade treatments, etc.
Develop grant/low interest loan program for rear entrance and façade improvements	Wilmington	Medium	Façade and entrance improvements are important for creating a desirable atmosphere. Wilmington should offer financial incentives to achieve this.
Improve perceived safety and security along corridor			
Install street lighting	Wilmington/DelDOT	High	Safety and pedestrian discomfort are obstacles to revitalizing corridor; streetlights would improve both issues. Cost approx. \$144,500
Remove obstacles to pedestrians			
Install sidewalk cellar doors over basement hatchways	Property/business owners	High	Pedestrian access is limited by protruding steps and railing. Cellar doors would widen usable sidewalk. \$25,000
Replace pipe railings and remove plywood edging	Property/business owners	Medium	Pipe railings and exposed plywood make corridor appear shabby. \$25,000
Improve the appearance of vacant buildings			
Develop property information system for vacant property	Wilmington	High	This is an important management tool to address vacant properties and needs to be maintained by the City and kept up to date.
Appoint Vacant Property Review Commission	Wilmington	High	Commission is responsible for identifying blighted vacant properties.
Work with property owners to bring vacant properties up to code and market properties	Wilmington/property owners/DEDO	High	Many property owners genuinely wish to improve and occupy their properties but may need technical expertise.
Install temporary or permanent treatments to improve the appearance of exposed walls	Wilmington/property owners	Medium	Exposed walls detract from desired atmosphere.
Rehabilitate vacant upstairs space as offices or apartments	Property owners	Medium	Vacant upstairs holds vast economic potential.
Develop incentive program for vacant and underutilized building rehabilitation	Wilmington	Medium	Rehabilitating upstairs into usable space may be costly.
Install consistent streetscaping			
Install consistent streetscaping as indicated in preferred concept plan	Wilmington/DelDOT	Medium	Locations and costs identified on Alternative Concepts in Appendix C.
Minimize the visual impact of trash storage			
Study the feasibility of taking over business trash collection to ensure quality and equitable service	Wilmington	Medium	Many City's offer this service, building in incentives for recycling and waste reduction.
Encourage the use of public art and landscaping			
Improve the façade of the parking garage, either with a mural or façade treatment	Wilmington/property owner	Medium	Parking garage is major destination on Shipley and currently detracts architecturally from surroundings.
Enhance park with new art, benches, landscaping	Wilmington	Medium	Park lacks identity and is a gathering point for the homeless.
Plant additional street trees and shrubs	Wilmington/DelDOT	Medium	Locations identified on Alternative Concepts in Appendix C. Cost \$23,750-\$76,000
Minimize the visual impact of parking			
Screen and define edges of surface parking lots	Wilmington	High	Locations identified on Alternative Concepts in Appendix C. \$11,000
Coordinate with Downtown Business Association			
Coordinate with DBA on improvements	Wilmington/DBA	High	Continue to involve DBA.

Long term (5+ years)	Action	Responsible Party	Priority	Comments	
	<i>Remove obstacles to pedestrians</i>				
	Establish centralized pay stations and remove individual parking meters or install double-headed meters	Wilmington	Medium	Meters clutter sidewalks but pay stations may face community opposition. Meters cost \$55,800, pay stations cost approx. \$6,000-\$13,000 per location plus \$4,500 for removal of existing meters	
	<i>Bury utilities</i>				
	Address utilities, either undergrounding or consolidating to reduce visibility	Wilmington	Low	Other corridors in Wilmington has a greater need for underground utilities. Cost \$8,075,000	
	<i>Improve the appearance of vacant buildings</i>				
	Acquire persistently vacant properties, either directly or through a downtown development corporation, for redevelopment	Wilmington	High	Blighted properties that continue to hamper revitalization efforts should be acquired through whatever means the City deems appropriate.	
	<i>Minimize the visual impact of trash storage</i>				
	Establish centralized trash collection locations, either between parcels or in parking lots	Wilmington	Medium	If other waste management strategies fail to address the problems, more extensive measures such as this may be needed. However this may face business opposition.	
	<i>Minimize the visual impact of parking</i>				
Develop surface parking with ground level retail incorporated into structured parking	Wilmington/parking lot owners	High	Surface parking represents valuable real estate for future development.		
<i>Coordinate with Downtown Business Association</i>					
Coordinate with DBA on improvements	Wilmington/DBA	High	Coordinate for DBA on long term decision-making.		

Appendix A

Visualizations and Alternative Concepts

- **Concept A** Improved street lighting, façade improvements and waste management, with existing sidewalks
- **Concept B** Improved street lighting, façade improvements and waste management, with brick/brick edged sidewalks
- **Concept C** Improved street lighting, façade improvements and waste management, with wide sidewalks on east side and additional east side street trees

VISUALIZATION "A"



BEFORE



AFTER- Concept A



AFTER- Concept B



AFTER- Concept C

VISUALIZATION "B"



BEFORE



AFTER- Concept A



AFTER- Concept B



AFTER- Concept C

VISUALIZATION "C"



BEFORE



AFTER- Concept A



AFTER- Concept B



AFTER- Concept C

BEFORE



AFTER- Concept B

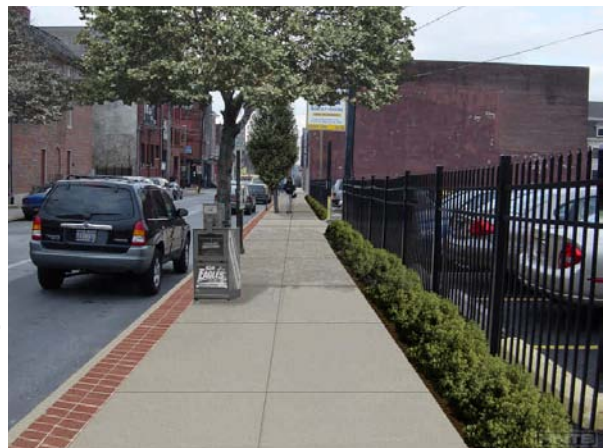


VISUALIZATION "D"

BEFORE

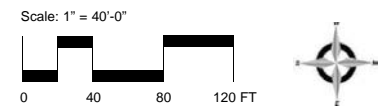
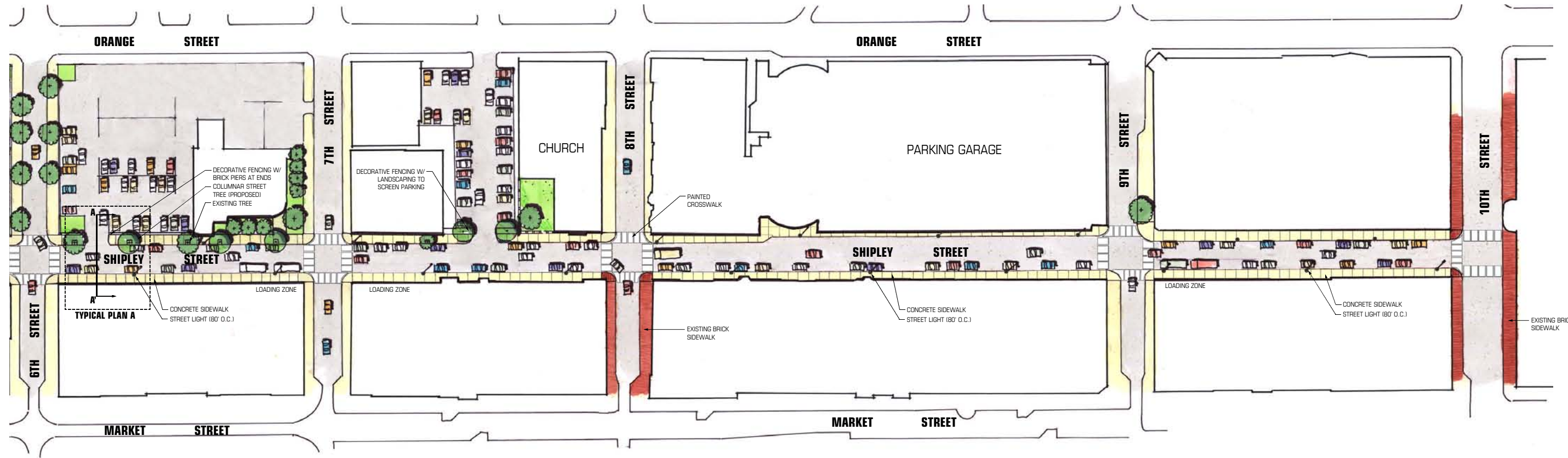


AFTER- Concept B



VISUALIZATION "E"

Shipley Street Improvements- Concept Visualizations
Wilmington, Delaware

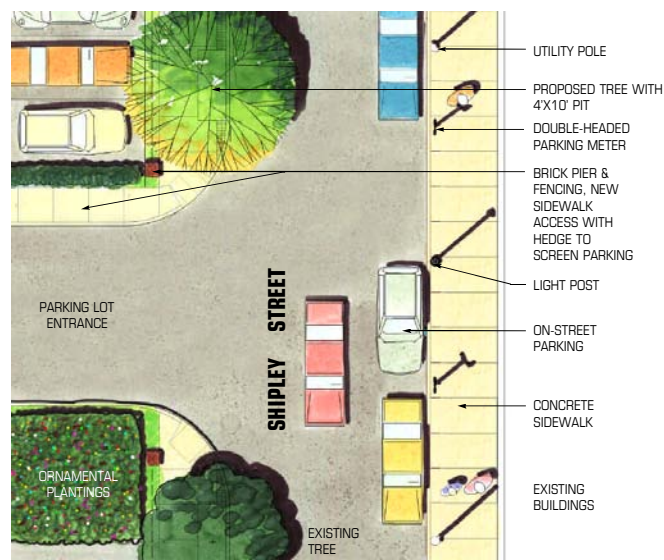


Desirable Condition
With an abundance of off-street parking in the Shipley Street corridor, it is very important to visually screen cars, as has been done in several cases within the corridor.

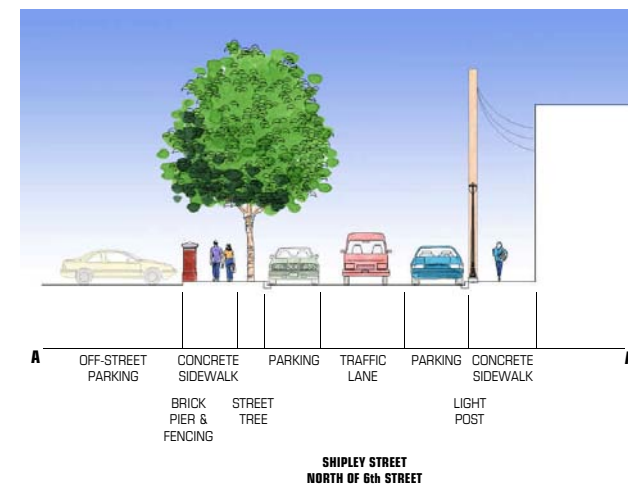
Undesirable Condition
Having surface parking adjacent to the pedestrian sidewalk with no defined edge is not only aesthetically displeasing, but dangerous as well in instances where vehicles must cross the sidewalk.

SURFACE PARKING EDGES

Surface parking along the Shipley Street corridor presents a variety of challenges. First, the parking breaks up the defined edge created by existing buildings. For this reason, an edge treatment, similar to the metal fencing and edge plantings that exist in some locations, should be implemented (see above). Brick piers can be located at key points as accents, while the new fencing helps to redefine the edge of the corridor in places where surface parking exists. This fencing will then help to visually screen the parking from the pedestrian environment, as well as to minimize pedestrian/vehicular conflicts caused by vehicles crossing the pedestrian way.



TYPICAL PLAN A
STREETSCAPE TREATMENT 2nd TO 10th STREET



TYPICAL SECTION A-A'
STREETSCAPE TREATMENT 2nd TO 10th STREET



Desirable Condition (proposed)
The existing ornamental railings behind Ships Tavern are aesthetically pleasing and reflect the building's architecture.



Undesirable Condition (existing)
Pipe railings and plywood edges add to the visual clutter along the sidewalk between 7th and 8th Streets.

RAILINGS

Existing pipe railings and plywood edging should be removed and replaced with a consistent decorative railing style. All railings should reflect the architecture of the building and should be black (exceptions shall be allowed when appropriate). Improving the rear entrances along the street will enhance the streetscape, while reducing the visual clutter that currently exists.



Desirable Condition (proposed)
Garbage should be contained in a consistent manner throughout the Shipley Street corridor. These existing containers are acceptable.



Undesirable Condition (existing)
Piling garbage in the sidewalk should be avoided at all times. Forcing pedestrians to walk around and through piles of garbage is unacceptable.

TRASH MANAGEMENT

Waste management is a major problem for the Shipley Street corridor. A program for managing and enforcing the removal of garbage from the street should be implemented. A standard trash container should be adopted and used by all properties along Shipley Street. Also, trash should be removed at least twice a week to minimize the opportunity for garbage to pile up. Those guilty of piling up garbage that does not fit into their containers prior to pick up day shall be fined appropriately.



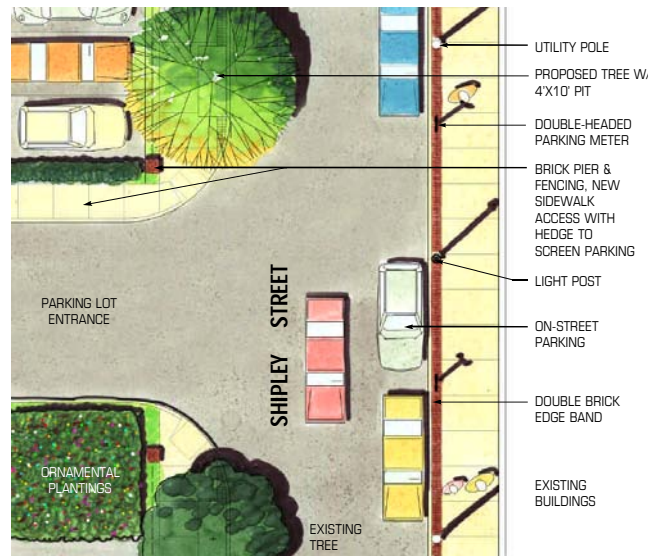
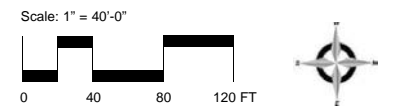
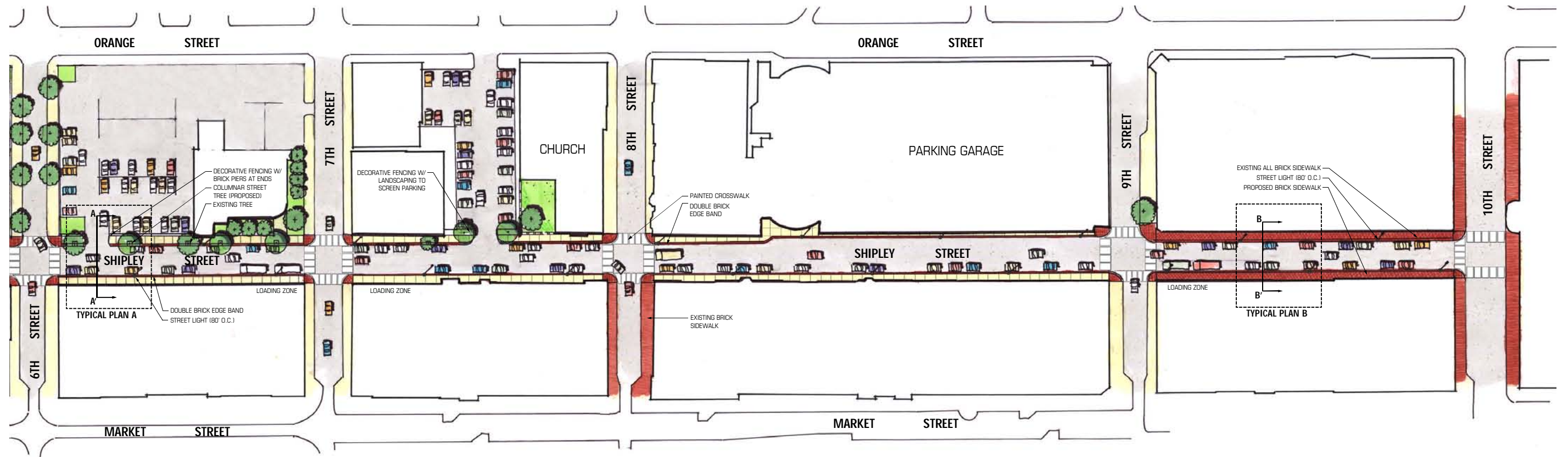
Desirable Condition (proposed)
Replacing existing sidewalks and curbs in poor condition will help to clean up the aesthetic look of the corridor. Replacing vault doors and dressing up building access points is also recommended.



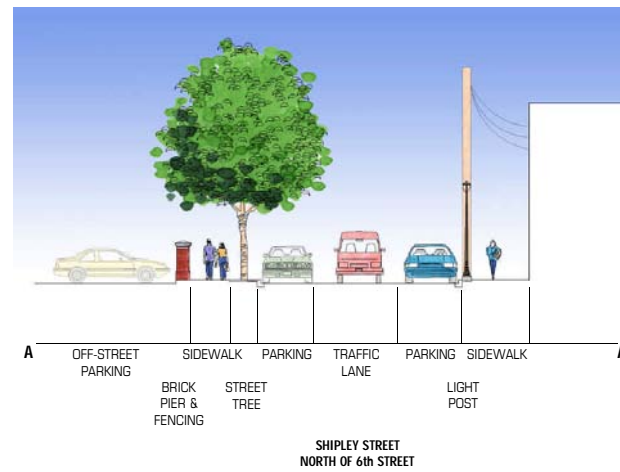
Undesirable Condition (existing)
Uneven and peacemeal sidewalks throughout the corridor are unattractive. Underground vaults and their access doors will likely need to be replaced or painted to provide a consistent treatment.

CONCRETE PAVING

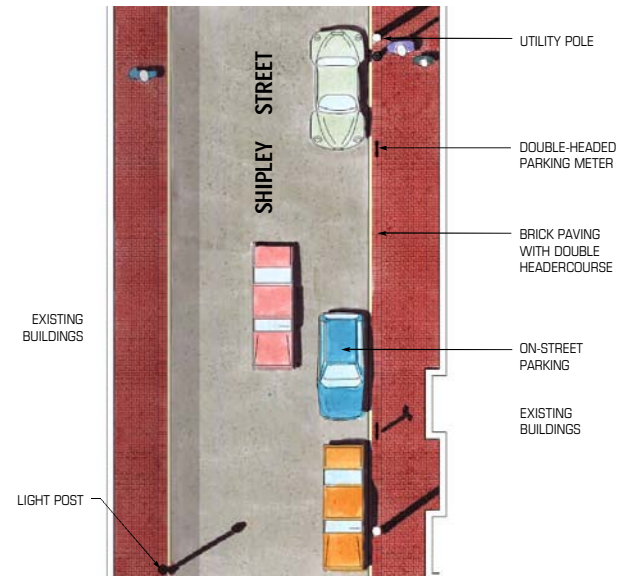
Replacing the existing concrete sidewalks and curbs will help in creating a uniform pedestrian zone throughout the Shipley Street Corridor. This uniform ground plane will help to visually tie together various elements, i.e. building facades, lights, parking meters, etc. The existing sidewalk is cluttered and interrupted with years of repairs and patches, all of which add to the already cluttered nature of the corridor. Replacing vault doors and dressing up building access points will help to further unify the corridor visually, while enhancing the pedestrian environment.



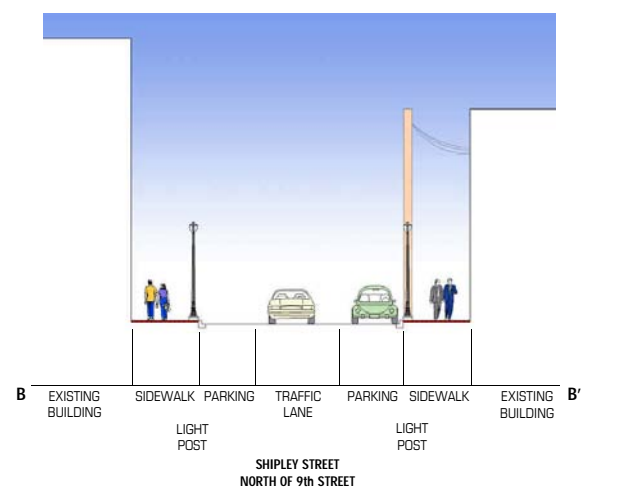
TYPICAL PLAN A
STREETScape TREATMENT 2nd TO 9th STREET



TYPICAL SECTION B-A'
STREETScape TREATMENT 2nd TO 9th STREET



TYPICAL PLAN B
STREETScape TREATMENT 9th TO 10th STREET



TYPICAL SECTION B-B'
STREETScape TREATMENT 9th TO 10th STREET



Desirable Condition (proposed)
The existing ornamental railings behind Ships Tavern are aesthetically pleasing and reflect the building's architecture.



Undesirable Condition (existing)
Pipe railings and plywood edges add to the visual clutter along the sidewalk between 7th and 8th Streets.



Desirable Condition (proposed)
Garbage should be contained in a consistent manner throughout the Shipley Street corridor. These existing containers are acceptable.



Undesirable Condition (existing)
Piling garbage in the sidewalk should be avoided at all times. Forcing pedestrians to walk around and through piles of garbage is unacceptable.



Desirable Condition
With an abundance of off-street parking in the Shipley Street corridor, it is very important to visually screen cars, as has been done in several cases within the corridor.



Undesirable Condition
Having surface parking adjacent to the pedestrian sidewalk with no defined edge is not only aesthetically displeasing, but dangerous as well in instances where vehicles must cross the sidewalk.



Desirable Condition (proposed)
Replacing existing sidewalks in poor condition and adding a brick accent band along the curb will help to unify elements throughout the corridor. Replacing vault doors and dressing up building access points is also recommended.



Undesirable Condition (existing)
Uneven and peaceable sidewalks throughout the corridor are unattractive. Underground vaults and their access doors will likely need to be replaced or painted to provide a consistent treatment.

RAILINGS

Existing pipe railings and plywood edging should be removed and replaced with a consistent decorative railing style. All railings should reflect the architecture of the building and should be black (exceptions shall be allowed when appropriate). Improving the rear entrances along the street will enhance the streetscape environment, while reducing the visual clutter that currently exists.

TRASH MANAGEMENT

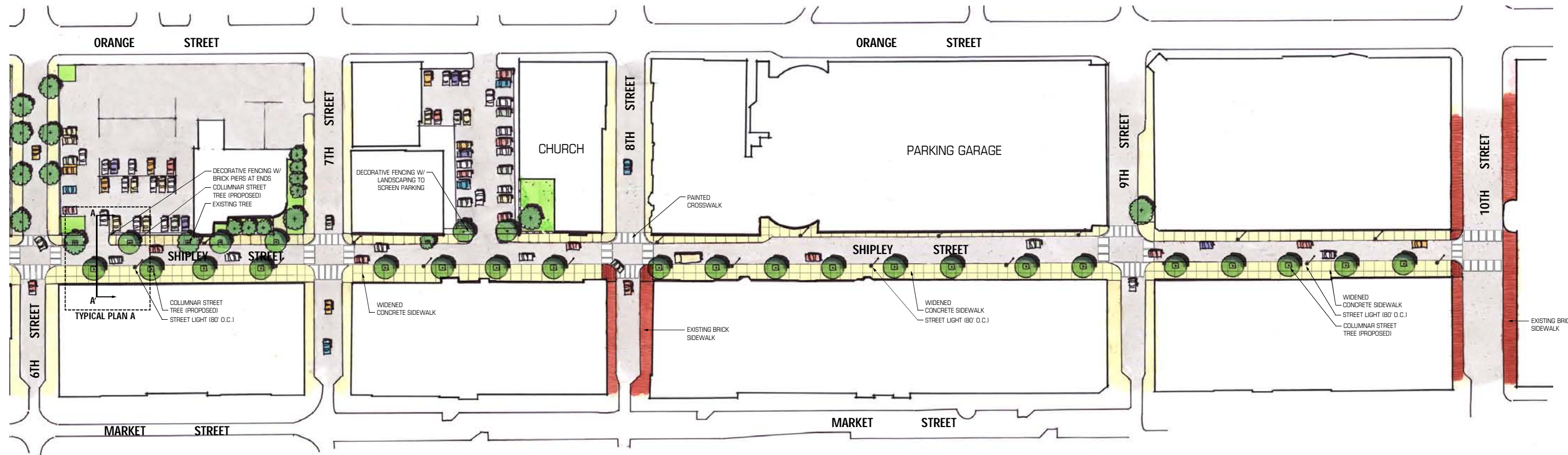
Waste management is a major problem for the Shipley Street. A program for managing and enforcing the removal of garbage from the street should be implemented. A standard trash container should be adopted and used by all properties along Shipley Street. Also, trash should be removed at least twice a week to minimize the opportunity for garbage to pile up. Those guilty of piling up garbage that does not fit into their containers prior to pick up day shall be fined appropriately.

SURFACE PARKING EDGES

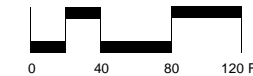
Surface parking along the Shipley Street corridor presents a variety of challenges. First, the parking breaks up the defined edge created by existing buildings. For this reason, an edge treatment, similar to the metal fencing and edge plantings that exist in some locations, should be implemented (see above). Brick piers can be located at key points as accents, while the new fencing helps to redefine the edge of the corridor in places where surface parking exists. This fencing will then help to visually screen the parking from the pedestrian environment, as well as to minimize pedestrian/vehicular conflicts caused by vehicles crossing the pedestrian way.

BRICK PAVING & ACCENTS

Building upon the existing brick paving at both ends of the corridor is ideal. To do this cost effectively, the introduction of a brick headercourse on the back edge of the curb is proposed. This band will unify the corridor through the use of a constant accent material, while also providing a "zone" for the placement of light posts, parking meters, signs, and utility poles. Having these elements located in a similar treatment will help to minimize the visual clutter that they create throughout the corridor. Relaying the concrete sidewalks will also create a uniform groundplane, which will further advance efforts to unify the corridor visually.



Scale: 1" = 40'-0"



Desirable Condition

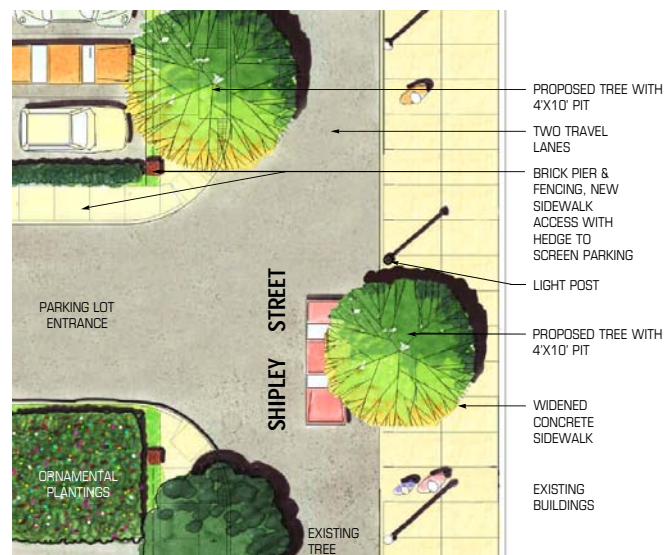
With an abundance of off-street parking in the Shipley Street corridor, it is very important to visually screen cars, as has been done in several cases within the corridor.

Undesirable Condition

Having surface parking adjacent to the pedestrian sidewalk with no defined edge is not only aesthetically displeasing, but dangerous as well in instances where vehicles must cross the sidewalk.

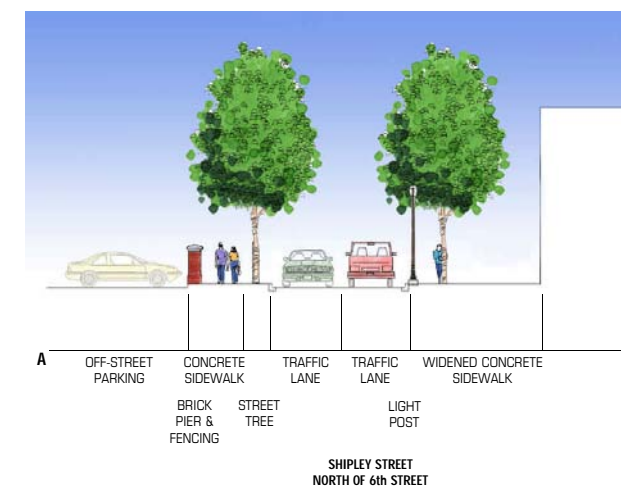
SURFACE PARKING EDGES

Surface parking along the Shipley Street corridor presents a variety of challenges. First, the parking breaks up the defined edge created by existing buildings. For this reason, an edge treatment, similar to the metal fencing and edge plantings that exist in some locations, should be implemented (see above). Brick piers can be located at key points as accents, while the new fencing helps to redefine the edge of the corridor in places where surface parking exists. This fencing will then help to visually screen the parking from the pedestrian environment, as well as to minimize pedestrian/vehicular conflicts caused by vehicles crossing the pedestrian way.



TYPICAL PLAN C

STREETSCAPE TREATMENT 2nd TO 10th STREET



TYPICAL SECTION C-A'

STREETSCAPE TREATMENT 2nd TO 10th STREET

Shipley Street Corridor

Jun-04

Wilmington, Delaware

**Preliminary Cost Estimate-
Concept Plan A**

Proposed Improvements

<u>Quantity</u>		<u>Item</u>	<u>Cost</u>	<u>Total</u>
40	EA	Street Lights	\$3,400.00	\$136,000.00
93	EA	Double-headed Parking Meter	\$600.00	\$55,800.00
9	EA	Brick Piers	\$3,500.00	\$31,500.00
20	EA	Street Trees	\$750.00	\$15,000.00
125	EA	Shrubs	\$70.00	\$8,750.00
110	LF	Ornamental Fencing	\$100.00	\$11,000.00
10,550	SF	Painted Crosswalk	\$3.00	\$31,650.00
0	SF	Brick Accent Band	\$18.00	\$0.00
0	SF	Brick Paving	\$18.00	\$0.00
368	SF	Sidewalk Detectable Warning System	\$30.00	\$11,040.00
41,365	SF	Concrete Sidewalk	\$4.35	\$179,937.75
5,135	LF	Poured in place Concrete Curbing	\$20.00	\$102,700.00
1	Lump Sum	Replacement/rehab of Vault doors	\$25,000.00	\$25,000.00
1	Lump Sum	Replacement of Railings	\$25,000.00	\$25,000.00
10,270	SF	Bit. Roadway Patching (Curb Installation)	\$5.75	\$59,052.50
5,135	LF	Sawcutting Hot Mix	\$2.00	\$10,270.00
50,000	SF	Variable Depth Milling and Overlay	\$1.25	\$62,500.00
Estimated Cost of Proposed Elements				\$765,200.25

Required Demolition

<u>Quantity</u>		<u>Item</u>	<u>Cost</u>	<u>Total</u>
385	SY	Removal of Existing Curbs	\$19.00	\$7,315.00
3,840	SY	Removal of Existing Sidewalk	\$19.00	\$72,960.00
25	EA	Removal of Cobra Heads from Ut. Poles	\$200.00	\$5,000.00
7	EA	Removal of Existing Lights	\$500.00	\$3,500.00
90	EA	Removal of Existing Parking Meters	\$50.00	\$4,500.00
1	Lump Sum	Utility Relocations	\$75,000.00	\$75,000.00
1	Lump Sum	Maintenance of Traffic	\$100,000.00	\$100,000.00
Estimated Cost of Demolition				\$268,275.00

<u>Quantity</u>		<u>Item</u>	<u>Cost</u>	<u>Total</u>
1	Lump Sum	Initial Expense (1.5%)	\$15,502.13	\$15,502.13
1	Lump Sum	Construction Engineering (2%)	\$20,669.51	\$20,669.51

Construction Cost of Improvements	\$1,069,646.88
Contingency 40%	\$427,858.75
Estimated Construction Cost	\$1,497,505.64

Say: \$1,500,000.00

Notes:

This estimate does not include any potential Right of Way cost.

Shipleigh Street Corridor

Jun-04

Wilmington, Delaware

**Preliminary Cost Estimate-
Concept Plan B**

Proposed Improvements

<u>Quantity</u>		<u>Item</u>	<u>Cost</u>	<u>Total</u>
40	EA	Street Lights	\$3,400.00	\$136,000.00
93	EA	Double-headed Parking Meter	\$600.00	\$55,800.00
9	EA	Brick Piers	\$3,500.00	\$31,500.00
20	EA	Street Trees	\$750.00	\$15,000.00
125	EA	Shrubs	\$70.00	\$8,750.00
110	LF	Ornamental Fencing	\$100.00	\$11,000.00
10,550	SF	Painted Crosswalk	\$3.00	\$31,650.00
5,210	SF	Brick Accent Band	\$18.00	\$93,780.00
7,990	SF	Brick Paving	\$18.00	\$143,820.00
368	SF	Sidewalk Detectable Warning System	\$30.00	\$11,040.00
28,165	SF	Concrete Sidewalk	\$4.35	\$122,517.75
5,135	LF	Poured in place Concrete Curbing	\$20.00	\$102,700.00
1	Lump Sum	Replacement/rehab of Vault doors	\$25,000.00	\$25,000.00
1	Lump Sum	Replacement of Railings	\$25,000.00	\$25,000.00
10,270	SF	Bit. Roadway Patching (Curb Installation)	\$5.75	\$59,052.50
5,135	LF	Sawcutting Hot Mix	\$2.00	\$10,270.00
50,000	SF	Variable Depth Milling and Overlay	\$1.25	\$62,500.00
Estimated Cost of Proposed Elements				\$945,380.25

Required Demolition

<u>Quantity</u>		<u>Item</u>	<u>Cost</u>	<u>Total</u>
385	SY	Removal of Existing Curbs	\$19.00	\$7,315.00
3,840	SY	Removal of Existing Sidewalk	\$19.00	\$72,960.00
25	EA	Removal of Cobra Heads from Ut. Poles	\$200.00	\$5,000.00
7	EA	Removal of Existing Lights	\$500.00	\$3,500.00
90	EA	Removal of Existing Parking Meters	\$50.00	\$4,500.00
1	Lump Sum	Utility Relocations	\$75,000.00	\$75,000.00
1	Lump Sum	Maintenance of Traffic	\$100,000.00	\$100,000.00
Estimated Cost of Demolition				\$268,275.00

<u>Quantity</u>		<u>Item</u>	<u>Cost</u>	<u>Total</u>
1	Lump Sum	Initial Expense (1.5%)	\$18,204.83	\$18,204.83
1	Lump Sum	Construction Engineering (2%)	\$24,273.11	\$24,273.11

Construction Cost of Improvements	\$1,256,133.18
Contingency 40%	\$502,453.27
Estimated Construction Cost	\$1,758,586.46

Say: \$1,760,000.00

Notes:

This estimate does not include any potential Right of Way cost.

Shipleigh Street Corridor

Wilmington, Delaware

**Preliminary Cost Estimate-
Concept Plan C**

Proposed Improvements

<u>Quantity</u>		<u>Item</u>	<u>Cost</u>	<u>Total</u>
40	EA	Street Lights	\$3,400.00	\$136,000.00
0	EA	Double-headed Parking Meter	\$600.00	\$0.00
9	EA	Brick Piers	\$3,500.00	\$31,500.00
57	EA	Street Trees	\$750.00	\$42,750.00
475	EA	Shrubs	\$70.00	\$33,250.00
110	LF	Ornamental Fencing	\$100.00	\$11,000.00
9,000	SF	Painted Crosswalk	\$3.00	\$27,000.00
0	SF	Brick Accent Band	\$18.00	\$0.00
368	SF	Sidewalk Detectable Warning System	\$30.00	\$11,040.00
62,165	SF	Concrete Sidewalk	\$4.35	\$270,417.75
5,335	LF	Poured in place Concrete Curbing	\$20.00	\$106,700.00
1	Lump Sum	Replacement/rehab of Vault doors	\$25,000.00	\$25,000.00
1	Lump Sum	Replacement of Railings	\$25,000.00	\$25,000.00
10,670	SF	Bit. Roadway Patching (Curb Installation)	\$5.75	\$61,352.50
5,335	LF	Sawcutting Hot Mix	\$2.00	\$10,670.00
34,000	SF	Variable Depth Milling and Overlay	\$1.25	\$42,500.00
480	CY	Borrow Type F	\$15.00	\$7,200.00
Estimated Cost of Proposed Elements				\$841,380.25

Required Demolition

<u>Quantity</u>		<u>Item</u>	<u>Cost</u>	<u>Total</u>
385	SY	Removal of Existing Curbs	\$19.00	\$7,315.00
3,840	SY	Removal of Existing Sidewalk	\$19.00	\$72,960.00
25	EA	Removal of Cobra Heads from Ut. Poles	\$200.00	\$5,000.00
7	EA	Removal of Existing Lights	\$500.00	\$3,500.00
90	EA	Removal of Existing Parking Meters	\$50.00	\$4,500.00
1	Lump Sum	Utility Relocations	\$75,000.00	\$75,000.00
1	Lump Sum	Underground Overhead Utilities	\$8,000,000.00	\$8,000,000.00
1	Lump Sum	Maintenance of Traffic	\$100,000.00	\$100,000.00
Estimated Cost of Demolition				\$8,268,275.00

<u>Quantity</u>		<u>Item</u>	<u>Cost</u>	<u>Total</u>
1	Lump Sum	Initial Expense (1.5%)	\$136,644.83	\$136,644.83
1	Lump Sum	Construction Engineering (2%)	\$182,193.11	\$182,193.11

Construction Cost of Improvements **\$9,428,493.18**
 Contingency 40% **\$3,771,397.27**

Estimated Construction Cost \$13,199,890.46

Say: \$13,175,000.00

Notes:

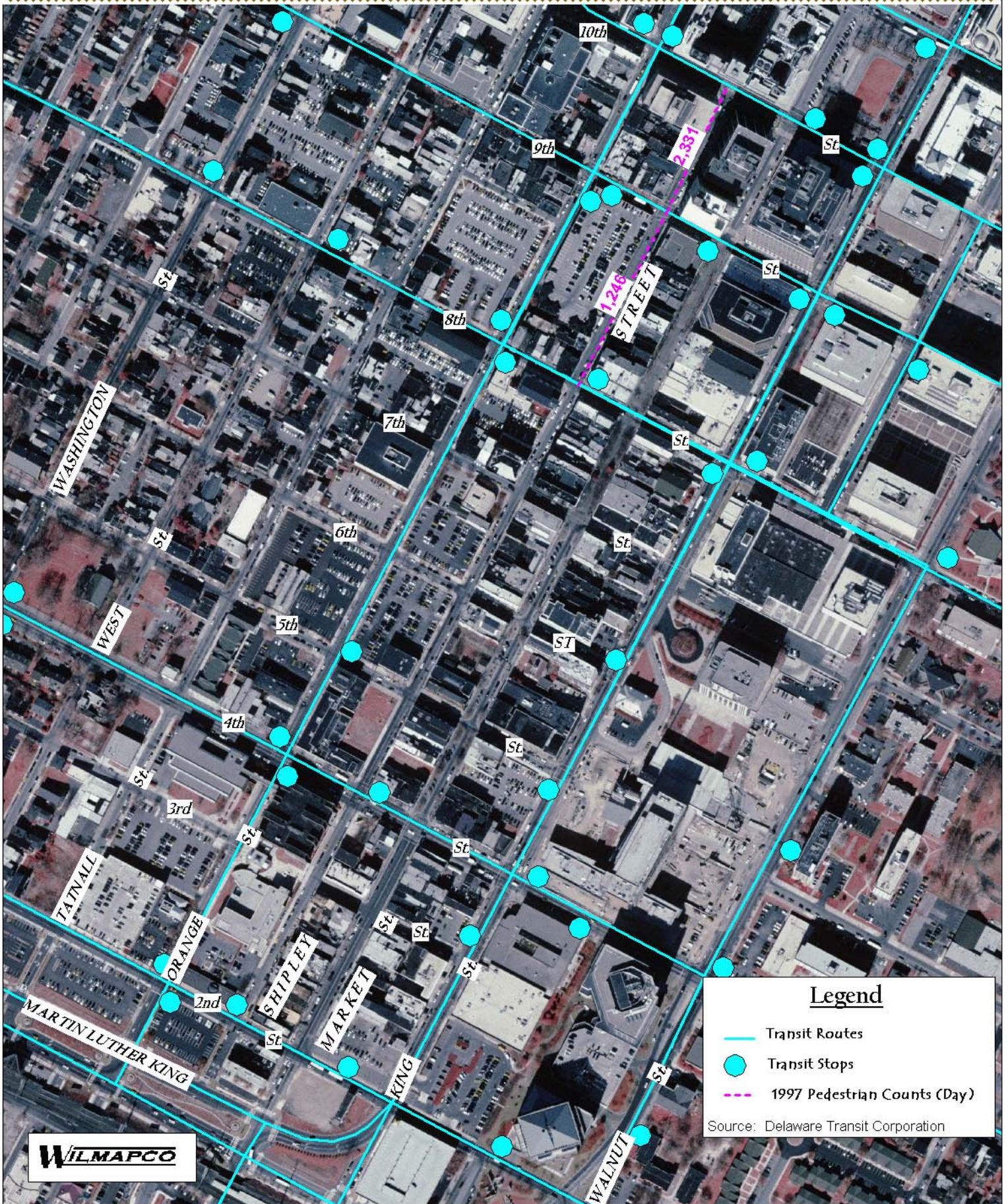
This estimate does not include any potential Right of Way cost.

Appendix B

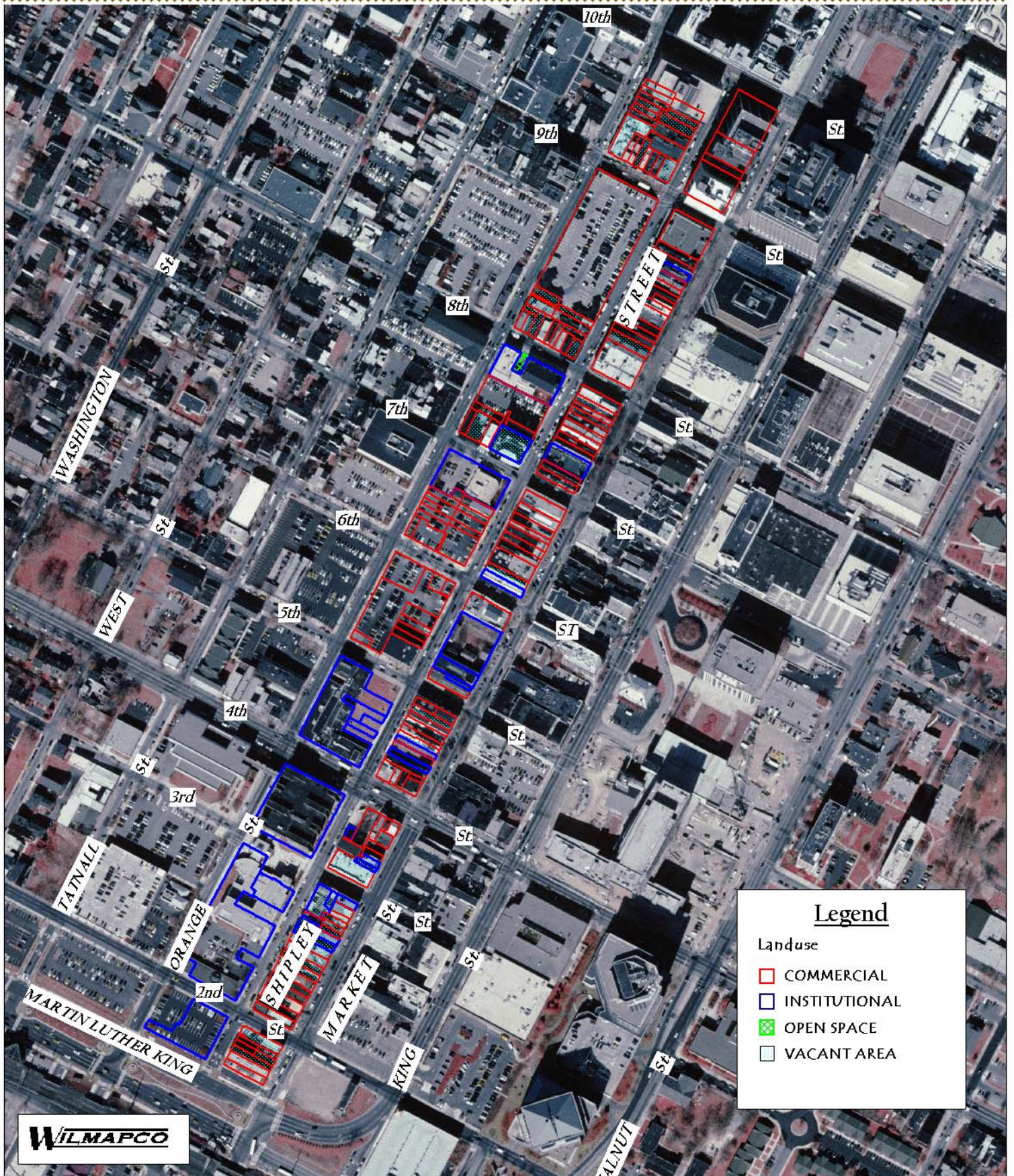
Mapping of existing conditions

- Transit and Pedestrian Counts
- Land Use
- Intersection Level of Service
- Population
- Accident Locations

Transit Routes, Transit Stops & Pedestrian Counts



Land Use



Source: Land Use, City of Wilmington; Vacancy, Kise Straw & Kolodner

Intersection Level of Service

LOS Based on 1999 Counts



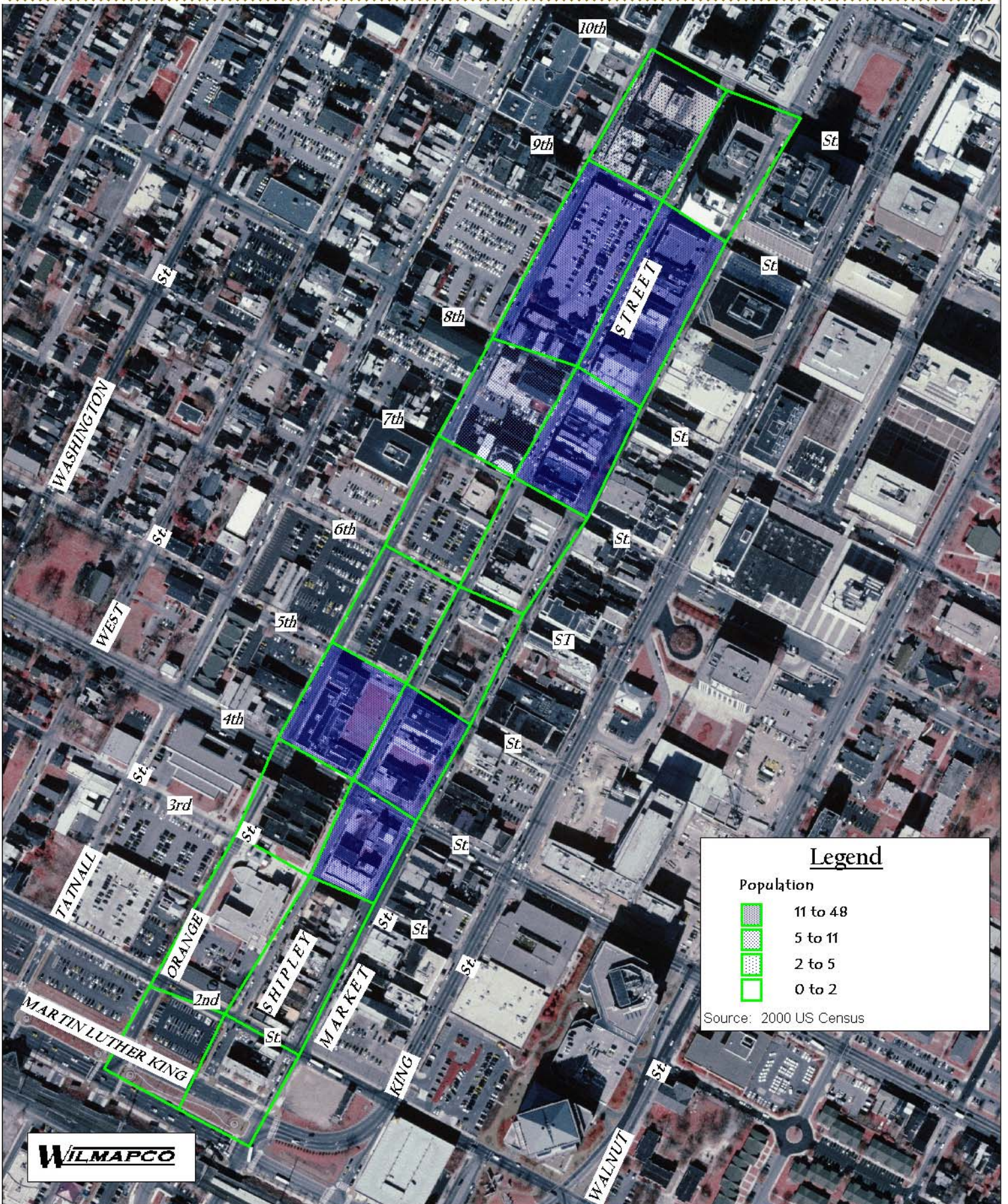
Legend

- AM Level of Service (LOS)
- PM Level of Service (LOS)

Source: DelDOT

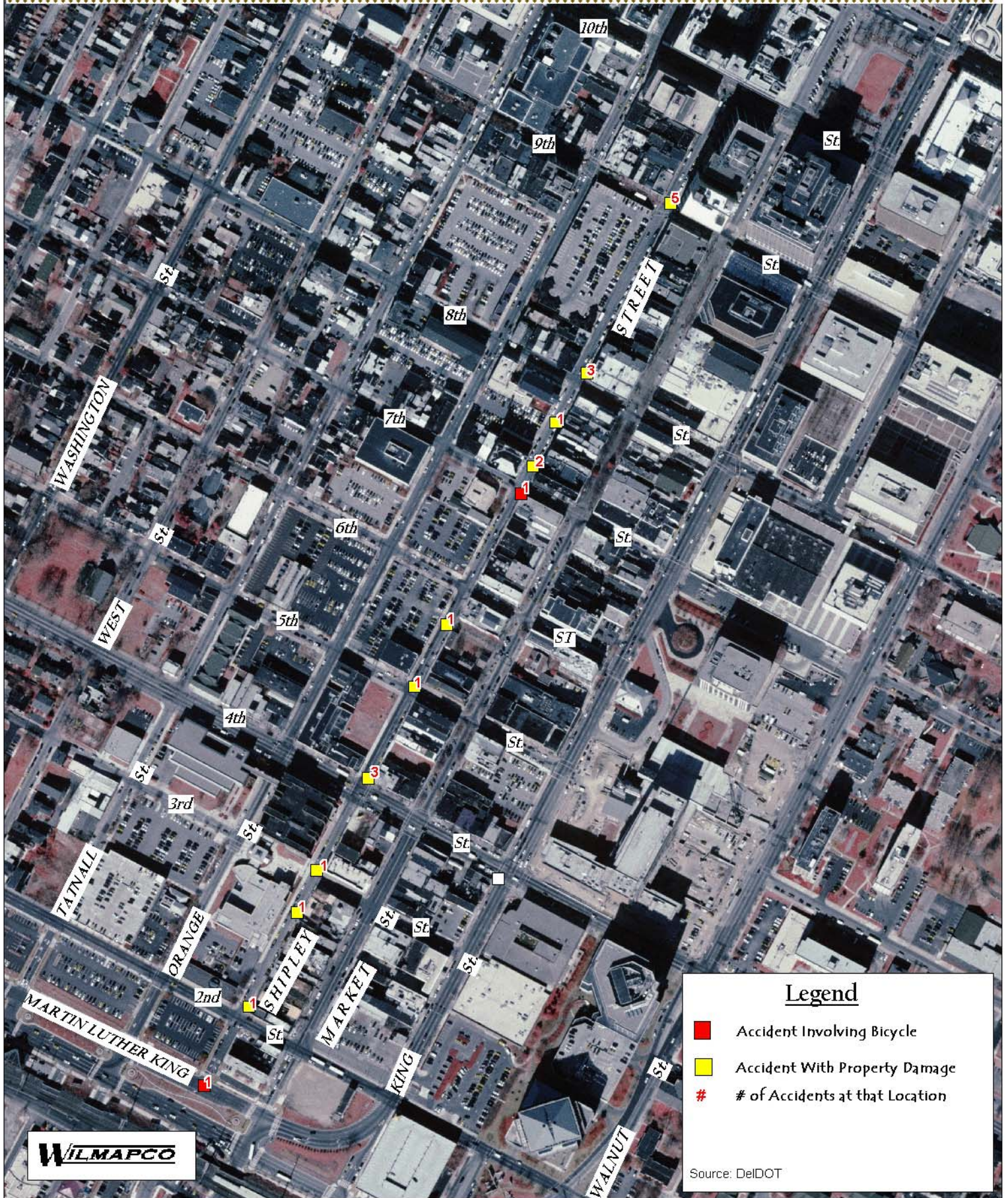
WILMAPCO

2000 Census Blocks



Accident Locations

January 00 to July 03



Appendix C
Public Comments and Outreach

ShIPLEY Street Revitalization Plan

1. How do you currently use Shipley Street (check all that apply)?

- 1** Main Entrance
- Employee Parking
- 2** Deliveries
- 3** Visit Businesses on Shipley Street
- Other⁵:
 - Alternate route south from 10th Street to MLK/I-95
 - 9th Street Book Shop borders 9th and Shipley Streets
 - Visit businesses on Market
 - As a traffic route
 - Exit from the Community Services Building
- 3** Back Entrance
- 4** Customer Parking
- 2** Trash Collection
- Live on Shipley Street

2. Please rate the following issues for Shipley Street on a scale of 1 to 5, with 5 being the greatest concern?

Issues	Not a problem				Major concern
	1	2	3	4	5
• Traffic congestion	6	3	2		
• Traffic volume	7	3	1		
• Pedestrian comfort and safety			3	3	5
• Traffic speeding	4	5	2		
• Availability of parking	2	1	7	1	
• Aesthetics of streetscaping	1			2	8
• Aesthetics of building facades			2	3	6
• Safety/security			3	5	3

- Other issues:
- Cleanliness
 - Sidewalk safety (basement entrances)
 - Delivery trucks

3. What is the greatest transportation issue that needs to be addressed along Shipley Street?

- Big trucks trying to turn onto Shipley Street from 10th Street
- Traffic calming
- Parking – availability to retail
- Wider sidewalks
- Pedestrian safety at night. Shipley is a dark street at night with the possible exception of the garage area at 9th St. A few years ago I was mugged on Shipley at 8th, after parking and on my way to the Grand. Darkness was the main culprit in addition to the lack of clean and safe team members on side streets. Also, friends have had car window smashed while dining.
- None – most use it as an alternative to Market. Traffic is light during the rush hours.
- Paving the street below 8th
- Pedestrian environment
- Appearance, trash collection, deliveries and restricting congestion
- Integrating the need for delivery vehicles with pedestrians

4. What transportation facilities and services are needed on Shipley Street?

1. Crosswalks and designated delivery areas
2. Pedestrian lighting as a functional element, sidewalk improvements
3. None
4. None
5. A system of attractive lighting
6. Street trees, planters and banners
7. Sidewalks trimmed in brick similar to Little Italy

5. Describe your vision of what you would like Shipley Street to be in the future?

- A fresh new look which is welcoming and pedestrian friendly. A street where utilities are beneath the ground and traffic calming is employed
- This is main city street with a strong history that is lacking today. A sense of place and history of the city should be the goals for refurbishing Shipley Street. Historical markers might be considered.
- Removal of surface parking lots, and a consolidation of many smaller lots to one large multi-story garage. Demolition of buildings without plans to build another structure immediately should be banned. The number of parking spaces should be capped. Entrances to businesses on Market from the Shipley side. Lighting of the building facades, and the use of white street lights; the amber lights create a harsh effect and make the surroundings appear more menacing. Promote conversion of unused space above retail as rentable living space. Shipley should be filled with apartment buildings and apartments above the current retail. Although, rentable retail space should be limited so as not to compete with Market Street, and create duplicate services.
- An efficient paved street with metered parking on one side and loading zones on the other for deliveries, pick-ups and trash collection. In other words, a basic back door street. No new landscaping needed. No concept changes necessary.
- Good pedestrian and vehicular access to economic activity nestled within and connecting the more primary Market Street and Orange Street corridors.
- An attractive, functioning street.
- To have a pedestrian orientation similar to west side of the 900 block that still allows businesses to get deliveries and function as businesses as you need to minimize deliveries on Market Street.
- Clean, safe, smooth streetscape like Little Italy, trees

Notes from January 7, 2004 “Walking Tour” Meeting

GENERAL ISSUES

- Connections to Market and Orange Streets (west/east) important and must be part of study area
- Shipley Street is used to reach Martin Luther King Blvd. (a relief route to King Street) and I-95
- Area has high visibility for visitors making appearance and safety important
- Functions as an alley for Market and Orange

PEDESTRIAN ISSUES

- Make pedestrian crossings more visible. Add signs
- Narrow east side – conflicts with utility poles, stairwells...
- Few pedestrians south of 8th Street
- Fire egress issues
- Vehicles violate crosswalks
- Utility poles
- In the area of 8th and 9th and Shipley streets, there is an East/West movement of freight and a North/South movement by pedestrians
- Underground vault exploration needed prior to sidewalk construction
- Building Access – constraints for streetscape
- 6th @ Springfield College - pedestrian/auto conflicts
- Pedestrians cross at all points - ill-defined crosswalks
- Maintain open sidewalk on at least one side during construction projects

LIGHTING, UTILITIES AND SECURITY

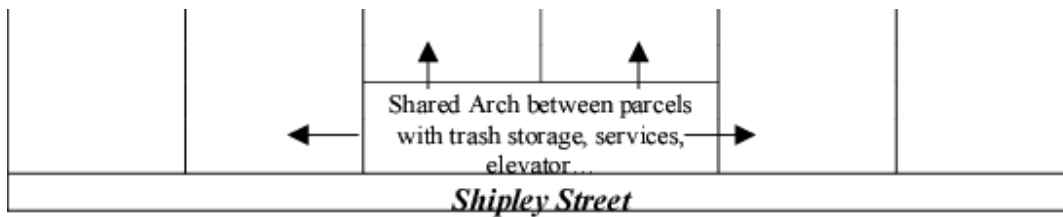
- Privately owned (not city)
- Additional lighting needed
- Some areas under surveillance
- Poles limit the width of sidewalks, particularly on the east side
- Clean and Safe Team patrols Shipley, yet area not perceived as safe

AESTHETICS/VISUAL ISSUES

- Consistent lighting needed
- Trash – improper containment – more bins encourage more trash
- Need proper trash containers, screening and enforcement
- Nice architecture obstructed by visual clutter (wires, poles, containers)
- Facade(s) improvement
- Parking garage is valuable location– short term visual improvements and possible redevelopment site (including more first floor retail)
- Retail won't succeed without more residents; area won't attract more residents unless it is cleaned up

LAND USE

- Vacant property on Market between 4th to 5th – new residential apartments with first floor retail will be under development soon
- Planning underway for mix use retail/apartments at 701 Shipley Street
- Ship Tavern under construction with apartments and retail
- Parking Garage being renovated, prime location makes idea for additional first floor retail
- Rumor exist about new parking garage planned for 900 block of Shipley, but City has not receive any plans for this
- Fire egress is a challenge for individual parcels, potential to share between adjacent parcels
- Presence of underground vaults, mechanical rooms, underground storage, etc. limit possible landscaping and work on sidewalks
- Shared utilities make undergrounding wires challenging



VEHICULAR USE

- Truck unloading – double parking an issue – no enforcement
- Enforcement issues in construction zones
- Meter parking – same cars all day
- Widen sidewalks on east side, provide on street parking on east side, and eliminate on street parking on west side
- Remove parking meters and replace with centrally located meter machine to reduce clutter
- Roadway surface poor quality
- Stacking issue onto Martin Luther King
- Illegal parking at intersections blocks visibility – possible bumpouts - watch loading zone conflicts
- Clearly identify loading zones
- 4th street signal backs up past 5th near Springfield College
- No perceived signals needed

TRANSIT ISSUES

- Shipley Street previously has bus service
- Merchants like it as a bus route because transit gives better customer access
- Consider trolley along Shipley Street

How to get involved:

- Attend the walking tour of Shipley Street, January 7 (10 a.m., Community Services Building, 100 W. 10th St). Tour will be held inside as a slide show if weather is unpleasant.
- Mail or fax back the attached survey.
- Schedule an individual interview. Call 302.737.6205.
- Attend the "Open House" Design Workshop on January 28 between 10 a.m. and 5 p.m. at the Delaware College of Art and Design (600 N. Market Street).

A partnership of WILMAPCO,
the City of Wilmington and
DeIDOT.

Creating a Safe, Attractive and Viable Commercial and Residential Environment

Shipley Street Revitalization Plan

Help improve Shipley Street!

Help us identify public streetscape improvements to support commercial and residential development initiatives along Shipley Street from 10th Street to Martin Luther King Boulevard.

Shipley Street serves as both the "back door" for businesses fronting onto Market Street and as a potential economic development corridor for the City. For existing businesses, it functions as a service road, or alley. Trash collection and utilities are along the corridor, as well as parking for customers and employees. Yet, the supply of vacant buildings, available parking and central location potentially make this neglected street attractive to new businesses.



The appearance of some parking has been improved with landscaping and attractive fencing.

Already, many examples of beautiful architecture, landscaping and streetscaping line the street which is home to restaurants, colleges, a church, and other businesses and nonprofit organizations. The Community Services Building and the Ships Tavern District are attractive anchors at either end of the area. However, vacant buildings, exposed garbage cans, and deteriorating building facades and infrastructure make this street inhospitable for pedestrians, business clientele, and potential new businesses.

For information call WILMAPCO at
302.737.6205 or visit www.wilmapco.org



A view looking north on Shipley Street

Possible strategies

By improving Shipley Street, we hope to:

- Enhance the commercial success of the corridor by ensuring a pleasant experience for business patrons
- Enhance the experience of pedestrians in the corridor
- Preserve and enhance historic buildings, streetscape, and architectural features
- Achieve these goals affordably so that business and property owners are benefited rather than burdened by the revitalization process

Possible strategies include:

- Enhancing rear building entrances and facades
- Removing obstacles for pedestrians
- Burying utilities
- Improving the appearance of vacant buildings
- Installing consistent streetscaping
- Minimizing the visual impact of trash storage
- Encouraging public art and landscaping
- Minimizing the visual impact of parking

Mark your calendars...

January 28, we will hold an open house design workshop between 10 a.m. and 5 p.m. at the Delaware College of Art and Design. This intensive planning session is designed to stimulate ideas and involve the public. This process offers you the unique advantage of giving immediate feedback to the designers while giving mutual authorship to the plan by all who participate. The community works side by side with professional architects, planners, and transportation engineers in a process that is quick, effective and fun.